

proposed DAIN WEST
SUBDIVISION

**URBAN & ARCHITECTURAL
DESIGN GUIDELINES**

August 24, 2020

1.0 INTRODUCTION

Dain West subdivision is a proposed mixed-use development located in Dain City at the south end of the City of Welland. Specifically, it is located south of Townline Tunnel Road (Highway 58) and east of Canal Bank Street and the Welland Recreational Canal.

The purpose of this document is to establish a practical and flexible design vision to create an attractive, pedestrian-scaled and cohesive neighbourhood, improving the overall health and vitality of the area and to provide design guidelines necessary to implement this vision. These Architectural and Urban Design Guidelines focus on the physical design of the community including the proposed character and treatment of built form, streetscapes, road network, pedestrian network and open space features. The development of a strong and attractive urban environment within the Dain West subdivision will help create a vibrant community that attracts people to live, work and play taking advantage of the unique characteristics and amenities within the area.

1.1 Site Description

The proposed Dain West subdivision includes +/- 74 hectares of land in the south end of the City of Welland; it is located between the Welland Recreational Canal and the Welland Shipping Canal, south of Townline Tunnel Road (Highway 58) and CN Rail corridor (Stamford Sub-Rail Line). Verbio Diesel Canada operates a biodiesel plant to the south-east of the subdivision lands and a CN Sub-Rail Line (GioRail) operates along the east property line. South of the site is an existing residential neighbourhood (the community of Dain City) with some minor commercial uses. West of the site, west of the Recreational Canal are agricultural lands with some rural residential uses.

There are also two large woodlots (the north woodlot and south woodlot) that have been incorporated into the plan and protected in two open space blocks not intended for development.



The strategic location of the proposed subdivision, between two canals, creates a unique opportunity within the City. The canals help generate a sense of identity and provide visual and physical amenity to the community creating unmatched opportunity for the provision of parks, trails and lookouts and strengthens the historic, current and future connection to these waterways.

1.2 Scope and Intent of Design Guidelines

These guidelines are intended to provide direction and not represent prescriptive requirements for the development. The City of Welland may waive individual guidelines for specific project components if an alternative better achieves the overall intent.

2.0 COMMUNITY VISION & OBJECTIVES

The proposed Dain West subdivision is envisioned as an inclusive and exciting development that enhances connections to and along the recreational canal while contributing to the local economy. The new community will provide walkable streets, access to green space, a variety of housing types, new parks and accessible (local) population related employment providing services, retail, office and other jobs and amenities to the community.

The following objectives are proposed:

1. To support the needs of the local community through careful design of an appropriate mix of house types and lot sizes suited to meet the evolving needs of families and individuals.
2. To provide integrated street and trail design maximizing opportunities for residents and visitors to experience the site and surrounding area on foot or bicycle. Making connections from the proposed neighbourhood trails to city and region-wide trails.
3. To promote a convenient and connected open space network and support the recreational needs of the community through careful design of public open spaces.
4. To harmoniously integrate a variety of well-designed homes and town homes into the existing neighbourhood to suit a range of community needs emphasizing the historic connection of the area to the adjacent canals and previous land uses on site.
5. To establish a high level of architectural quality in the neighbourhood that is coordinated and appropriate to its context. Architectural styles for the proposed Dain West community are influenced by a number of the sites physical characteristics and historical past including its location at the south end of Welland adjacent to agricultural lands and between two active canals, the Recreational Canal and the Shipping Canal.

Some of the architectural styles to be used are derived from Late Victorian, Prairie, Craftsman, Farmhouse, Georgian, Colonial, Edwardian and Greek Revival. These styles favour a more family rural tradition and seek a modern reinterpretation of these styles respecting their guiding proportions and principles adapted to current building techniques, roof shape and materials. Slavish replication of period detail is not sought. The proposed architectural styles are influenced

by several homes in the Welland area, such as “The Bridgetenders House” which remains in one of the oldest residential areas of Welland and is surrounded by mostly Victorian architecture. The “Brookfield-Cupido House” is another notable architectural inspiration within Welland, comprising of Italianate and Edwardian Classical styles.

6. To ensure that building entrances are visible and strongly articulated to give every resident a sense of address and pride of home. Porches should be provided to encourage an active and populated street.
7. To establish an appealing streetscape within and at the edges of the development by minimizing the visual impact of garages and surface parking areas on the streetscape acknowledging that new housing types each come with different architectural and lot parameters.
8. To provide a harmonious variety of dwelling models, elevations, sizes, building materials and exterior colours, which will achieve a diverse expression avoiding monotony in the streetscape, yet achieving a unified environment.
9. To ensure that non-residential buildings meet design standards of the highest quality, are consistent and coordinated with the architectural style of the residential buildings and represent appropriate development for the community.
10. To provide guidance to the designers and builders to ensure the highest possible standard of design is reasonably met.
11. To ensure construction follows the approved design program.
12. To create a complete community within close proximity to services.
13. To foster a safe and secure pedestrian friendly environment.

3.0 ADMINISTRATION OF THE GUIDELINES

These guidelines are intended to encourage and maintain a high level of architectural and urban design quality throughout the period of development of the subdivision. Minor amendments to the Urban & Architectural Design Guidelines to reflect specific concerns may be made by Empire Communities and their designers.

The zoning by-law as approved and amended formally controls the extent of building and massing. Conformity with these guidelines, however, does not override or reduce the minimum legal restrictions of the by-law.

3.1 Street Trees

The following guidelines should be applied to the provision of street trees:

- Residential streets are to be planted with a single row of street trees. The target of 1 tree per dwelling will be encouraged where space allows.
- Native species are encouraged.
- Where headlights may shine on housing units a coniferous tree or trees may be planted to mitigate impacts.
- Street trees will generally be required to be planted on the public right-of-way.

- Street trees will be planted with alternating species variety (to prevent spread of disease), and sufficient space for healthy root growth and to achieve large mature canopies.

3.2 Gateways & Community Entrances

Gateways and community entrances provide a sense of arrival and place and act as markers to identify the beginning of something new. Gateways and entry features can take the shape of signs, markers, plantings or public art.

Gateways and community entry features, if proposed, should be situated in strategic locations on Canal Bank Street and at major intersections providing access to the proposed subdivision. These features should be scaled appropriately based on where they are being proposed. For example, a feature along a trail or sidewalk, geared towards people, should be smaller and more detailed as pedestrians and cyclists will be able to approach and interact with the piece.

Monumentation can be used to outline and elevate the importance of natural and/or physical elements that define the site; for example, the Welland Recreational Canal or the significant woodland features being preserved on site.

3.3 Street Furniture

Street furniture occurs within the public right-of-way and typically includes street lights, mailboxes, seating/benches, waste receptacles, public signage/sign blades, utility elements, fencing etc. A palette of high-quality street furniture can help distinguish the neighbourhood and strengthen the social role of streets and other outdoor public spaces. Street furniture will be coordinated and established through discussions with the County and developer.

The following guidelines should be applied:

- Street furniture will be provided for the safety and convenience of users at appropriate locations. Including pedestrian/roadway lighting, waste receptacles, benches, signage etc.
- Utility infrastructure should be located away from open space frontages and community landmarks where possible (i.e. entry features).
- Street lighting will be in accordance with the City of Welland standards.
- Above ground infrastructure should be located and designed to be compatible with the surroundings, organized and visually minimized.

4.0 PARKS & OPEN SPACE SYSTEM

4.1 Trails, Parks and Parkettes

The subdivision will be served by two new parks including one large neighbourhood park (“Canal Bank Park”) that is approximately 2.0 hectares and a linear park that is approximately 2.2 hectares. “Canal Bank Park” takes advantage of now available land adjacent to the canal created through the realignment of Canal Bank Street and will provide a unique opportunity for residents to access the

waterfront. The linear park is located along the CN Railway trunk line at the east side of the plan. This park provides four significant functions: (1) it is a passive park for the community with meandering trails; (2) it acts as a wildlife corridor allowing safe passage of wildlife between two woodlots; (3) it provides space for a noise berm designed to help mitigate noise generated by the railway line; and (4) provides a safety barrier that helps to deter people and animals from being on the tracks.

Park elements should be designed to take advantage of the unique physical traits of its strategic location. “Canal Bank Park” should be designed as an active place to congregate, play and enjoy the water. The linear park should be designed as a passive park where users can calmly enjoy nature and stroll through the community.

Where possible, parks and other open spaces should be connected by a network of trails through greenspaces and/or the public rights-of-way.

The following guidelines should be applied to the provision of trails, parks and other open space:

- A naturalized approach to design (layout and planting) shall be adopted in the development of the linear park.
- Provide appropriate barriers to unwanted vehicular traffic and provide safety barriers to ensure safety along the waterfront.
- Canal Bank Park shall be designed to support:
 - Structured and unstructured use.
 - Access to the waterfront.
- Where appropriate, playgrounds should be designed as a focal point.
- Design elements of the parks and trails are encouraged to acknowledge and highlight the industrial past of site.
- The extent of asphalt walkways throughout Canal Bank Park should be minimized and unnecessary duplication with other walkways or sidewalks should be avoided.
- Planting (trees, shrubs, and grasses) shall comprise species that are tolerant of urban conditions with an emphasis on native species.

4.2 Stormwater Management Ponds

The stormwater management pond will be integrated as a positive and safe feature of the community.

The following guidelines should be applied:

- A naturalized approach to design (layout and planting) shall be adopted in the development of storm water management facilities.
- Native plant material will be utilized where possible. Plant materials should be extended to the pond perimeter to provide shade opportunities for the pond surface.

- Where street frontage occurs adjacent to the pond block, additional landscape features such as seating, decorative paving and accent planting may be provided.
- Where possible, provide the opportunity for overlooks/viewing areas that emphasize the naturalized open space areas. Provide paved or naturalized surfaces, safety features (i.e. railings) and furniture where appropriate/necessary.

5.0 COMMUNITY ARCHITECTURE

5.1 Design Principles for Residential Architecture

The following design principles apply to all residential lots:

- All housing will be designed to reinforce a pedestrian friendly neighbourhood.
- The front face of all dwellings with street frontage should be located close to the minimum building setback but with adequate variation provided to ensure flexibility and prevent rigidity in the streetscape.
- Garages should be subordinate to the overall home façade. The front façade of the dwelling should directly relate to the street and visually dominate the garage (i.e. recessed garages). Where this is not possible, special consideration should be taken to the architectural design of the unit.
- Wraparound porches are encouraged on corner lots. Porch encroachments into front and exterior side yards should be permitted by the zoning by-law to enable these features which will be an essential component of neighbourhood character.
- A variety of architectural expression among publicly exposed facades should occur within each street block.
- Main entrances should face the street and be connected to the sidewalk and/or driveway by a hard surface walkway.
- The scale, height and massing of a typical street block should seamlessly connect to the adjacent street, creating a well-balanced, human scale massing which encourages pedestrian activities.



- Vehicular access should be consolidated wherever possible. This may include approaches such as the pairing of driveways in an effort to maximize the opportunity for on-street parking.

5.1.1 Low Density Development, Architectural Guidelines

The following architectural guidelines apply to all low-rise residential dwellings:

5.1.1.1 Themes and Architectural Styles

The architectural character should not detract from the immediate neighbourhood context and should be familiar and relevant to the architectural tradition in Welland. Elements including porches, bay windows and other building projections should be included as typical features within this community. A variety of elevations suggest an evolved quality rather than an instant “cookie cutter” look and a modest scale of home celebrates simple living rather than ostentatious expression.



A variation of elevations and materials provide a cohesive architectural theme

5.1.1.2 Townhome Design

Townhomes are comprised of individual units grouped together into a larger architectural form. The massing and design of each townhouse block rather, than the individual units should be reviewed based upon the design merits of the block. Appropriate design variety between adjacent blocks should be provided. Elements such as varied wall planes and roof lines should be incorporated to help break up larger block massings.



Exterior end units of townhomes that face open space or parks should incorporate building projections, gables and architectural features where permissible to maximize opportunities for overview and safety.



Townhouse Corner units should incorporate architectural features that address both fronting streets and with a flanking elevation that includes windows consistent with the front elevation. Wrap around porches, gables and other architectural features are encouraged to be used along both building frontages.

5.1.1.3 Through Lots

Through lots shall be mindful of the need to produce an attractive and safe streetscape on both frontages of the building, providing for both vehicular and pedestrian safety and landscaping opportunities. **Corner designs of townhouse units should incorporate architectural features addressing both fronting streets**

Garages in the rear yard should match the main dwelling through vernacular, massing, materials, and color. In locations of high public exposure, garages should be designed to the same level as the main dwelling and finished with materials compatible with the front. Main entrances for corner units should be located on the flankage elevation to create a building appearance consistent with the adjacent detached housing.



Outdoor amenity areas should be located in the rear yard and may be adjacent to the driveway, between the dwelling and garage, or provided in the form of a balcony/patio located above the garage, and adequate privacy screen should be provided from the public

right-of-way (at the rear of the lot).

5.1.1.4 Covered Front Porches

Covered front porches, porticos and stoops are typical of local traditional housing styles. Models should feature a strong and highly visible entrance through the use of porch or portico.

5.1.1.5 Front Entrances

Front entrances should be visible from all approaches and should be designed in detail to match the architectural character of the home.

Entrance porticos and porches should be aligned with the dominant unit feature (gable, window peak, etc.) while framing and featuring the unit entry.

A variety of front door styles is to be provided to give each unit a unique and personal entrance, in character with the building's architecture. Traditional decorative front door casings and surrounds and other features are encouraged but not mandatory.

5.1.1.6 Roof Designs

The designs should provide a variety of architecturally appropriate traditional roof slopes and types.

Visible side slopes of front roofs such as front gables, should have roof pitches of a min. 8:12. Less visible front to back roof pitches can be reduced to 6:12. In certain circumstances, a front to back roof may be lower than a 6:12 pitch to meet design objectives.

5.1.1.7 Exterior Materials

Approved vinyl siding, clay masonry brick, concrete brick or combinations of brick and siding shall be used as the dominant materials on all elevations of all buildings.

Each home and townhouse block should introduce a harmonious variety of accent cladding materials such as:

- Precast features and bands;
- Window surrounds/casings;
- Crezone panels with trim boards;
- Stone;
- Board and Batten; and
- Vinyl shingle and shake designs.

5.1.1.8 Exterior Colours

Non-masonry cladding materials should harmonize with the dominant brick colour. A contrasting accent coloured cladding on gable panels or window surrounds is permitted. Colour coordinated trim boards of at least 4" width should be used at all non-masonry corners and around all openings in non-masonry panels.

On each townhouse block, eaves, soffits, fascias and frieze boards, where used, shall match or be complimentary in colour. One brick colour should be used per block.

Roof colours should reflect the tones of traditional roof materials and should be dark in nature. Very light-coloured roofs such as white or light grey should be avoided.

5.1.2 Priority Lot, Architectural Guidelines

In addition to the above, additional guidelines apply to priority lots, **corner lots** and **terminus lots**. For illustration, the below priority lot site plan depicts priority lots on a section of the proposed draft plan (Figure 1). The priority lot guidelines shall apply across the site.

5.1.2.1 Corner Lots

Three faces of corner lots are prominently visible to the passing public. As such, these faces must all be carefully designed.

A creative mix of brick and siding may be permitted on corner lots. Accent windows and surrounds, gables, bay windows, porches and other features are encouraged and may be constructed of other acceptable accent materials, such as stone and stucco. Upgraded design treatment consistent with the front elevation shall be provided to the flanking and rear elevations of all corner lots.



5.1.2.2 Terminus Lots

Terminus lots include lots at the end of a T-intersection, lots at the end of cul-de-sacs, and lots on elbow curves.

These lots shall use complementary elevations and materials creating a focal statement. Bay windows, gables, porches and feature entrances are encouraged on these lots.

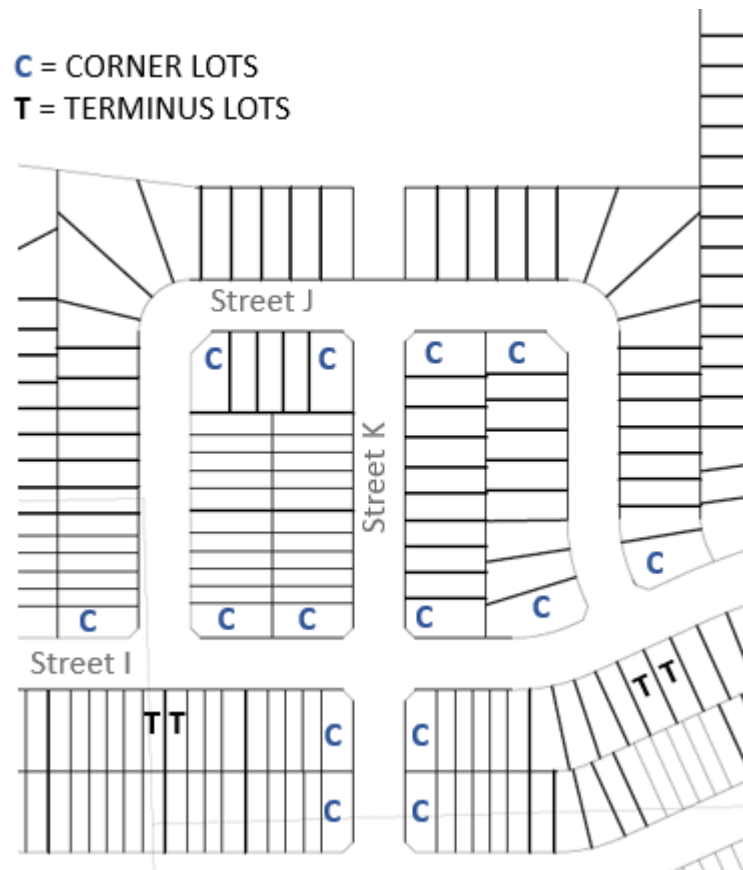


Figure 1: Priority Architectural Lots

5.1.3 Siting Requirements

5.1.3.1 Model and Elevation Repetition

The following guidelines should be considered during model selection and siting:

- The siting of identical elevations side by side or directly opposite the street should be avoided
(see "A" on Figure 2)
- A minimum of (2) other models shall separate a model with the same style (elevation)
i.e. Model 1 -B sold on lot 2 cannot be sold again until lot 5
(see "B" on Figure 2)
- A model may be sold next to the same model if the style is different. A maximum of 3 elevation styles of the same model may be sited next to each other
(see "C" on Figure 2)
- An identical style (elevation) should not be sited more than 3 times in a row of 10 lots.
(see "D" on Figure 2)
- On corner lots models with the same style (elevation) should not be sited directly opposite each other.

(see "E" on Figure 2)

5.1.3.2 Colour Selection

The following guidelines should be considered during colour selection:

- Identical colour packages must be separated by at least two (2) dwelling units (i.e. if colour package "X" is on lot 2, it cannot be selected again until lot 5).
(refer to lot numbers on Figure 2)
- Homes with the same exterior colour package shall not be sited directly opposite each other.
- On corner lots, the exterior colour package shall be different than the package across the street.

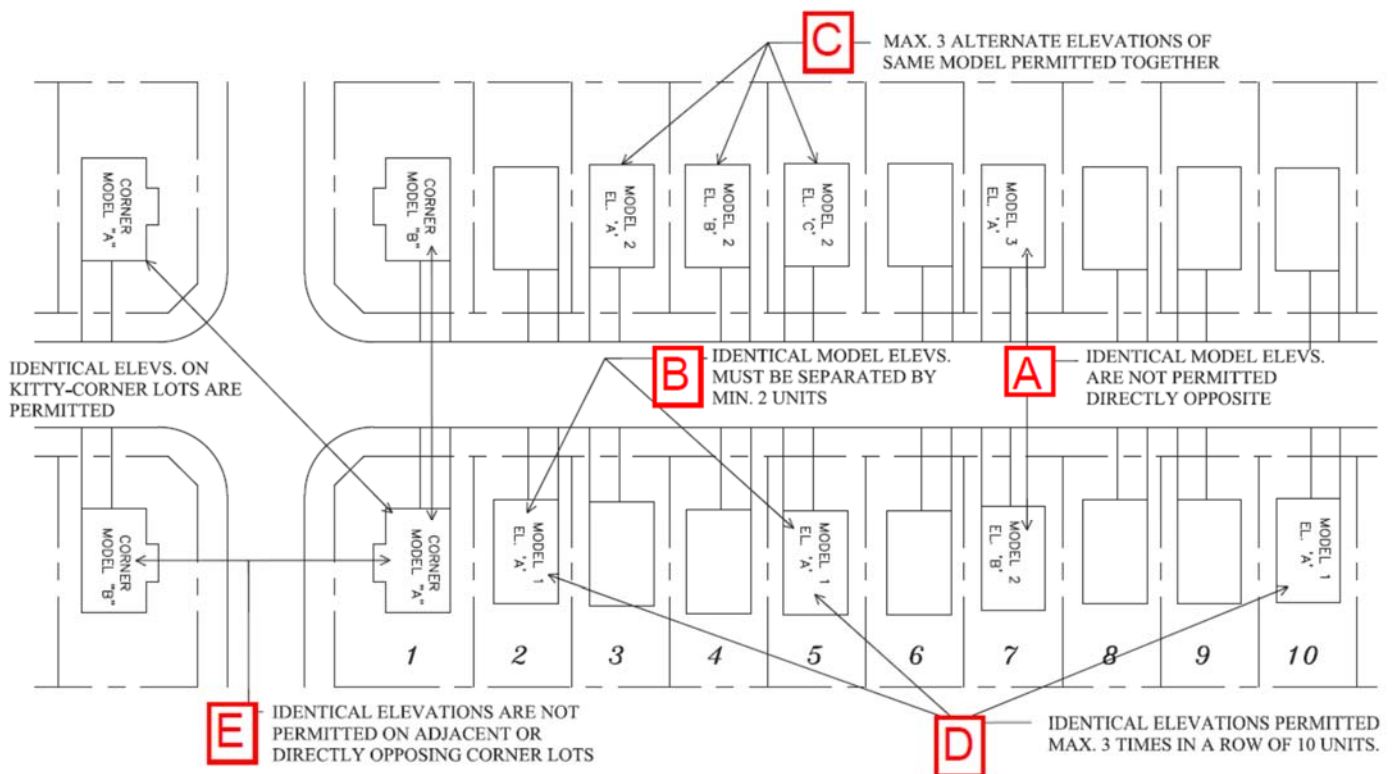


Figure 2 – Model Repetition and exterior colours

5.2 Principles for Commercial

New buildings should be context sensitive, respond to the characteristics and features of the individual site and be developed to minimize the visual impacts on adjacent residential uses.

5.2.1 Architectural

- Design and detail the building to be compatible with adjacent built form through massing, materials and / or other design strategies, with consistent architectural detailing on all publicly exposed elevations.
- Design excellence should be evident in all aspects of building design, including the choice of materials, proportion and massing, detailing colour, lighting, and all other areas of design.
- Building design and architectural styles are encourages which reinforce the built form character of Dain City (Welland).
- Buildings and other elements should be organized onsite to respect adjacent land uses and public open spaces.
- Locate buildings close to the street edge, oriented to address all streets and intersections.
- Articulate building elevations with changes in plane to break up long, continuous stretches.
- Appropriate transitions shall be incorporated into the design so they relate to adjacent residential dwellings.
- If and where possible, vary long roofscapes to incorporate visual interest.
- Provide visual interest through design, articulation and fenestration. Large unarticulated wall surfaces that face public spaces shall be avoided unless meant for large-scale display (mural, signage etc).
- Provide expansive storefront windows for views to activities inside, creating interest for passersby and to serve as a visual connection to the outdoors.
- Provide space for higher pedestrian volumes, patios and spillover space where applicable.
- Screen roof-top mechanical equipment to ensure a high-quality streetscape presence.

5.2.2 Pedestrian Access

- Emphasize the pedestrian experience by ensuring safe and comfortable walkways. Linkages and connections between commercial areas and adjacent neighbourhoods shall be provided.
- Primary pedestrian entrances should be visible and easily accessible from the public sidewalk.
- Tree planting, soft landscaping, street furniture and surfacing material changes should be used to define, improve and reinforce pedestrian routes.
- Coverage over pedestrian entrances for weather protection will be encouraged.
- Include sidewalks and clear pavement markings throughout the site to ensure pedestrian safety.
- Barrier-free sidewalks should be provided, where feasible, leading directly from the public street, transit stops and/or parking areas to the main building entrances.

5.2.3 Parking & Vehicular Access

- Vehicular access should be appropriately integrated into the design of the building massing and public realm, in such a way that it supports the design concept and does not detract from the design of the building elevation and the overall character of the street.
- Screen loading, service and garbage storage areas by building, screen walls and/or landscaping coniferous planting for year-round screening of negative views.
- Primary entrance to the mixed-use block should be from a local road; however right-in, right-out movement may be considered on Canal Bank Street.
- Minimize conflict between pedestrian walkways and drop-off locations and vehicular access by locating vehicular access away from main entrance to buildings.
- Avoid parking between the building and the street where possible.
- Coordinate snow storage with landscape design.
- Primary vehicular routes through the site should be clearly defined with the use of signage, curbing, bollards, lane painting and/or paving material.
- Parking surfaces shall be paved with hard surface material.

5.2.4 Signage & Lighting

- Provide commercial signage that is clearly illuminated with accent lighting complementary to the design of the building.
- Grade related commercial signs should be used at key vehicular entrances.
- Signage shall not cause negative impacts to residential areas.
- Exterior lighting should not cause negative impacts to residential areas.
- Exterior lighting shall be located and designed to provide a safe environment for pedestrians.

These Urban and Architectural Design Guidelines provides guidance on community design for the proposed Dain West Subdivision including direction on design of the mixed-use block, dwellings, parks and open spaces and the public realm ensuring a successful built form that reinforces and compliments the existing Dain City community.